



Development Services Commission
PUBLIC MEETING INFORMATION REPORT

DATE: Tuesday, June 16, 2026

APPLICATION TYPE: Official Plan and Zoning By-law Amendment (the “Applications”)

OWNER: 1421122 Ontario Limited and 1421121 Ontario Limited (the "Owner")

AGENT: The Planning Partnership (the “Applicant”)

LOCATION | WARD: 4080, 4084, and 4088 Highway 7 | Ward 3

FILE NUMBER: PLAN 25 141750

PREPARED BY: Hailey Miller, RPP, MCIP, Senior Planner

REVIEWED BY: Stephen Corr RPP, MCIP, Manager, Central District
Stephen Lue, RPP, MCIP, Senior Development Manager

PROPOSAL: To permit a development comprised of three, 12 storey residential buildings, with 701 units and 539 parking spaces (the “Proposed Development”)

PURPOSE

This preliminary information pertains to the Applications submitted by the Owner and contains general information on the applicable Official Plan policies and the identified issues and should not be taken as Staff’s opinion or recommendation.

PROCESS TO DATE

Staff deemed the Applications complete on April 21, 2026. The 120-day period, as set out in the Planning Act before the Owner can appeal to the Ontario Land Tribunal for a non-decision, ends on August 19, 2026.

NEXT STEPS

- The Statutory Public Meeting is scheduled for June 16, 2026
- Recommendation Report for consideration by the Development Services Committee (“DSC”)
- In the event of approval, adoption of the site-specific Official Plan Amendment and enactment of the site-specific Zoning By-law Amendment
- Submission of future Site Plan Control applications and potentially Draft Plan of Condominium application depending on tenure

BACKGROUND

Figures 1 and 2 show the 1.4 ha (3.5 ac) Subject Lands located on the north side of Highway 7 East and east of Warden Avenue, currently developed with two car dealerships with associated surface parking. Figure 2 shows the surrounding land uses.

The Proposed Development includes three residential buildings with the following (see Figure 4)

- Building Height: 12 storeys (45 m)
- Residential Gross Floor Area: 62,398 m² (671,651 ft²)
- Floor Space Index (FSI): 4.4
- Dwelling Units: 701
 - 1 bedroom – 527 units (75%)
 - 2 bedroom – 111 (16%)
 - 3 bedroom – 63 units (9%)
- Vehicle Parking Spaces: 539 spaces (469 residential, 70 visitor)
- Bicycle Parking Spaces: 620

The Owner proposes to amend the Markham 2014 Official Plan (the “2014 Official Plan”) to permit the Proposed Development

- Current Designation: Residential Mid Rise
- Permitted Uses: Townhouse including back-to-back townhouse, small multiplex containing 3 to 6 units, stacked townhouse, apartment building, shared housing (small scale, large scale, and supervised care home), buildings associated with day care centres, places of worship, and public schools.
- Maximum Height and Density: The designation permits a maximum height of 6 storeys and density of 2.0 FSI. Area and Site-Specific Policy 9.19.8 e) requires that the height shall be minimum 2 storeys and maximum 3.5 storeys for the Subject Lands.
- Proposal: The Owner proposes to amend the 2014 Official Plan to add a site-specific policy to increase the maximum permitted height and density.

A Zoning By-law Amendment application is required to permit the Proposed Development

The Proposed Development is subject to By-law 2024-19, as amended, as shown in Figure 3.

- Current Zone: Residential Mid Rise Two (RES-MR2)
- Permissions: Apartment dwelling, multiple dwelling, multiplex dwelling, townhouse dwelling, home child care, home occupation, and shared housing (small scale, large scale, and supervised care home).
- Proposal: The Owner proposes to add site specific development standards including, but not limited to, maximum building height, maximum floor space index, minimum parking spaces, and minimum landscape strip.

Staff identified the following preliminary list of matters that will be assessed through the review of the Applications, including other matters and issues, and addressed in a future Recommendation Report to the DSC

- a) Conformity and Consistency with Provincial, and York Region and City Official Plan**
 - i) The appropriateness of the proposed Official Plan and Zoning By-law amendment to permit the Proposed Development.
 - ii) Review of the Proposed Development in the context of the existing policy framework and with consideration of the emerging policies of lands on the south side of Highway 7, within the Markham Centre Secondary Plan.

- b) Community Benefits Charges (“CBC”) By-law**
 - i) The Applications will be reviewed in consideration of the City’s CBC By-law.

- c) Parkland Dedication and Other Financial Contributions**
 - i) The Applications will be reviewed in consideration of the appropriate amount of parkland dedication and/or cash-in-lieu of parkland, public art contribution, and other financial contributions.

- d) Affordable Housing**
 - i) The Applications will be reviewed in consideration of Provincial, Regional, and City polices to ensure that the Proposed Development provides an appropriate level, range, and mix of unit sizes and types to meet the City’s affordable housing goals.
 - ii) Incorporating appropriate affordable housing, purpose-built rental, seniors housing, and family friendly units.

- e) Allocation and Servicing**
 - i) The availability of water and sanitary servicing capacity for the Proposed Development must be identified. If servicing allocation is unavailable, the Subject Lands will be zoned with a Holding Symbol, which will be removed once servicing capacity has been confirmed.

- f) Review of the Proposed Development will include, but not limited to, the following:**
 - i) Examination of whether the height, density, built form, and land uses proposed are appropriate.
 - ii) Evaluation of the compatibility with existing and planned development within the surrounding area.
 - iii) Sun shadow analysis and the impacts to the immediate surrounding areas.

iv) Transportation assessments within and around the Subject Lands including, but not limited to, capacity analysis, mobility safety review, access management, parking study, multi-modal connections (walking, cycling, transit), and grid network analysis.

g) External Agency Review

i) The Applications must be reviewed by York Region and any applicable requirements must be incorporated into the Proposed Development.

h) Required Future Applications

i) The Owner must submit applications for Site Plan Control, should the Applications be approved, to examine the appropriate landscape, site layout, snow storage areas, building elevations, amenity areas, and the coordination with the planned development to the east.

ACCOMPANYING FIGURES

Figure 1: Location Map

Figure 2: Aerial Photo

Figure 3: Area Context and Zoning

Figure 4: Conceptual Site Plan

Figure 5: Conceptual Elevations (South)

Figure 6: Conceptual Elevations (East)

Figure 7: Conceptual Elevations (West)

Figure 8: Conceptual Elevations (North)

Figure 9: Conceptual Rendering

Figure 1: Location Map

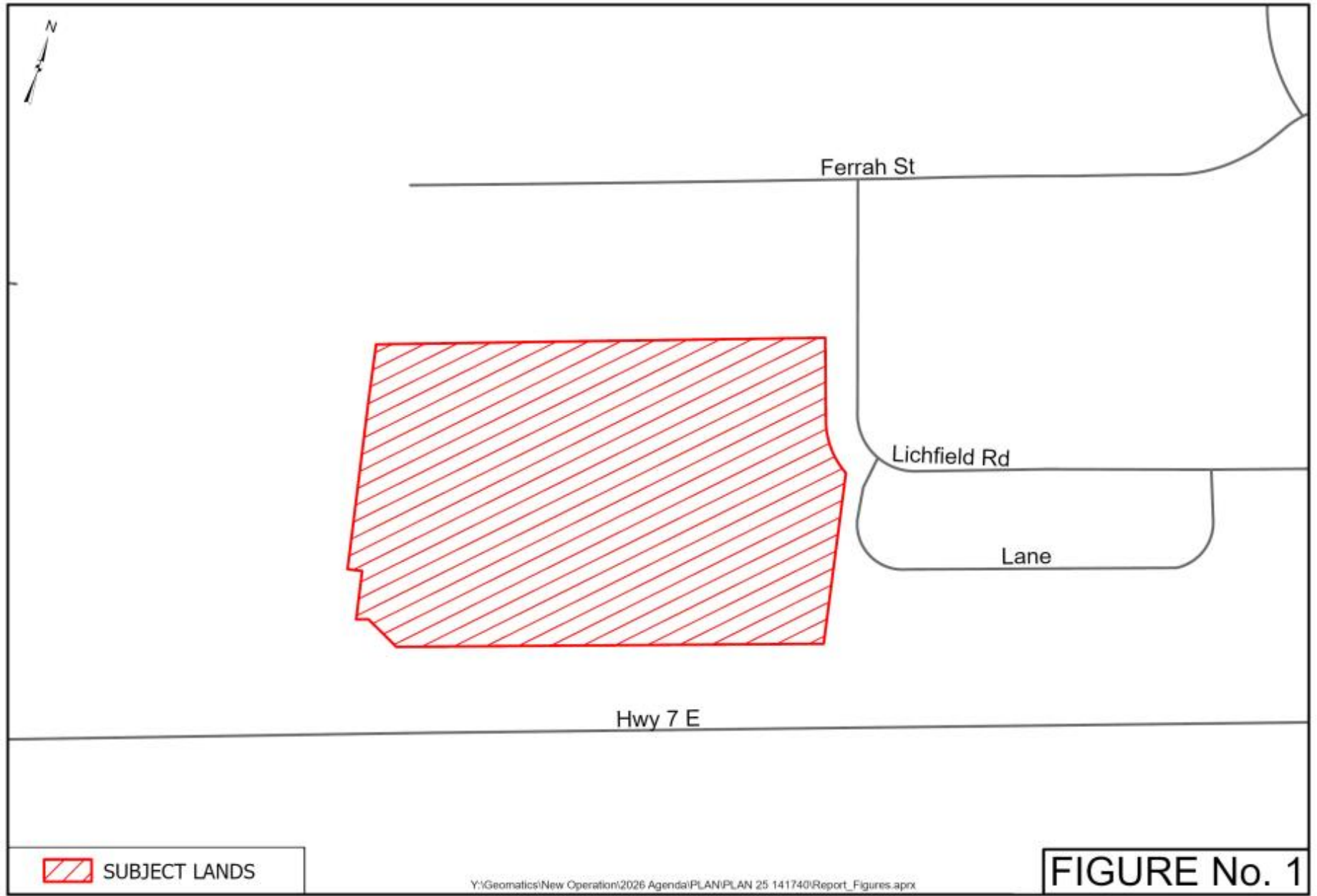


Figure 2: Aerial Photo



 SUBJECT LANDS



Figure 3: Area Context and Zoning

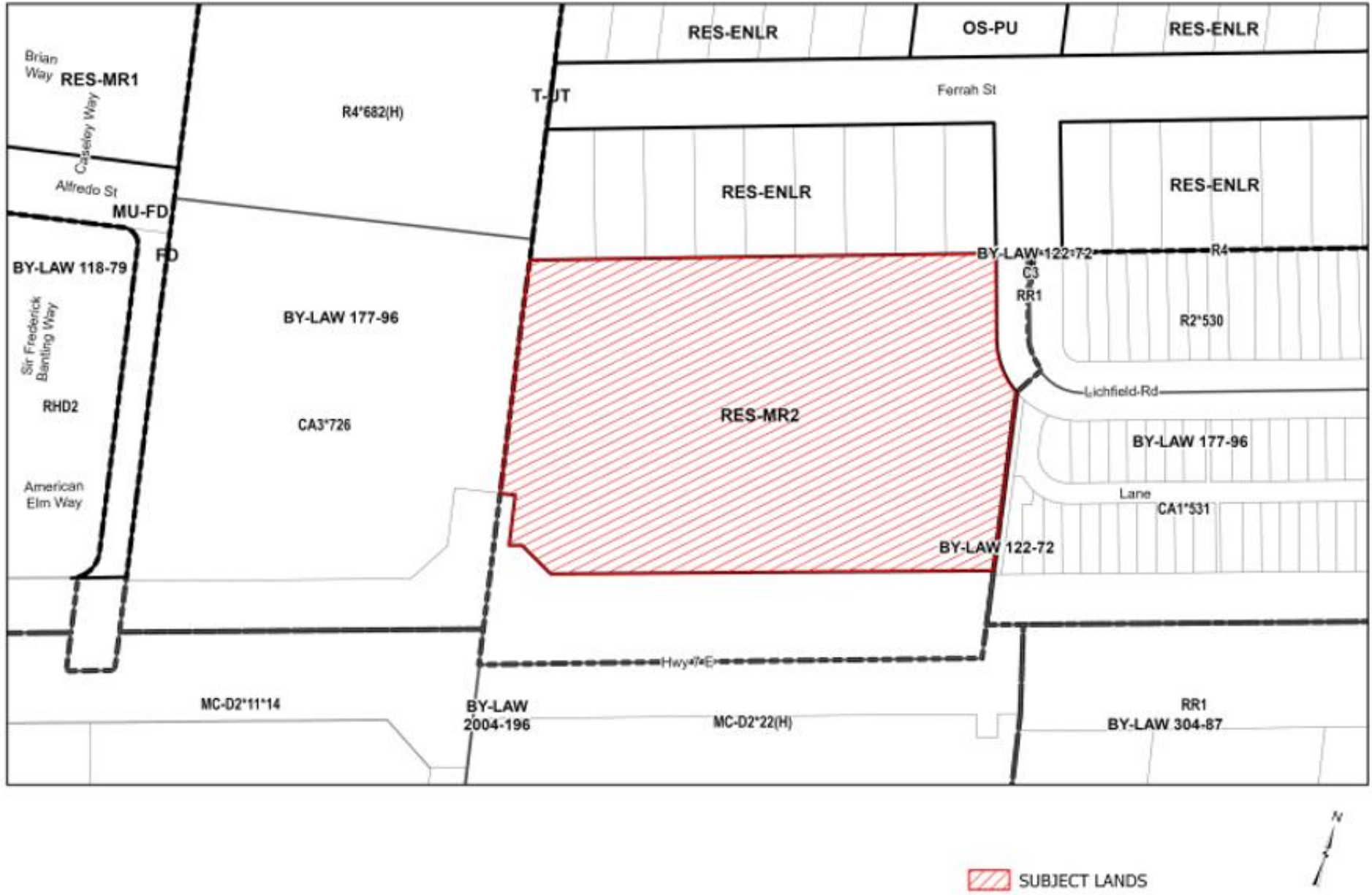


Figure 4: Conceptual Site Plan

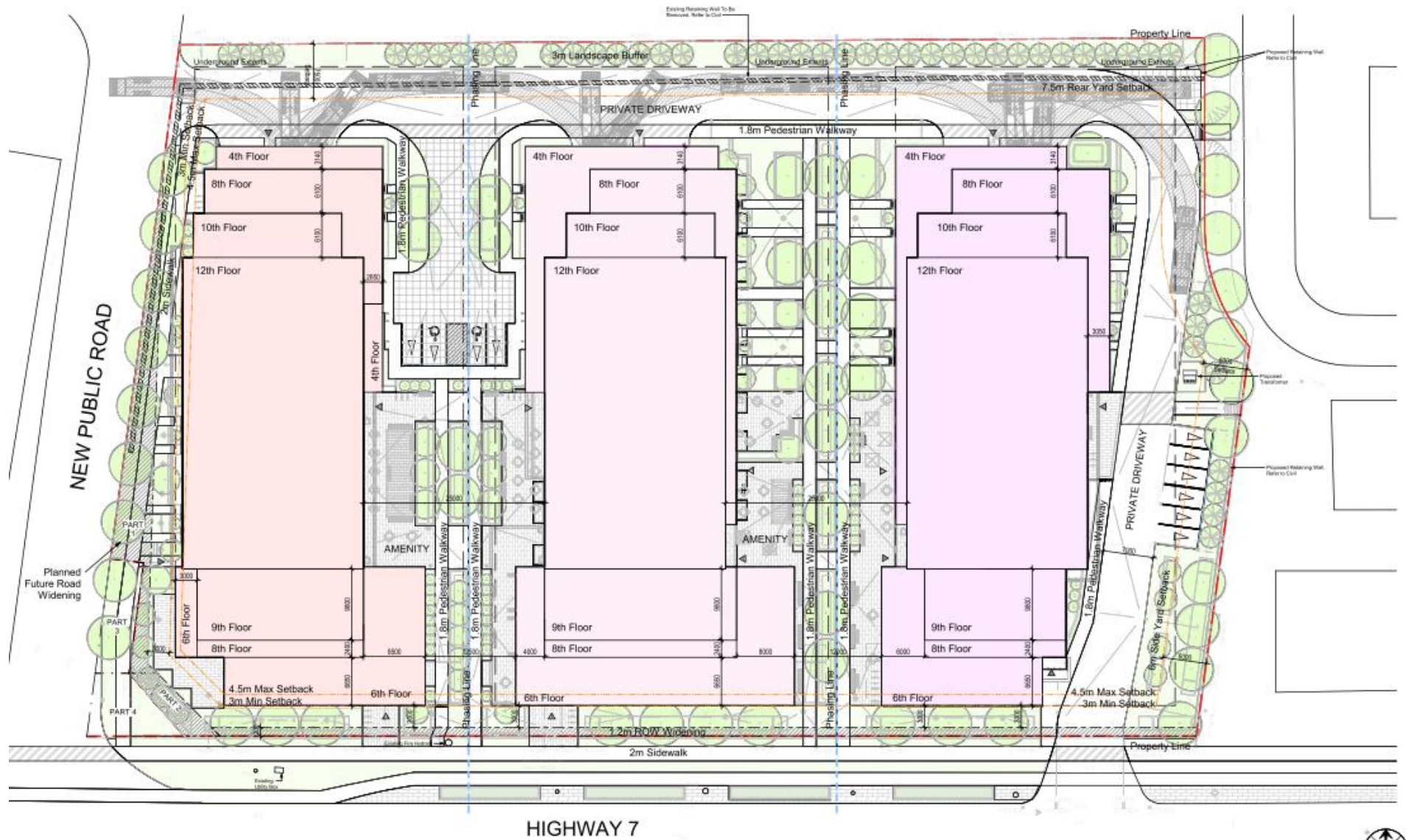


Figure 5: Conceptual Elevations (South)



Figure 6: Conceptual Elevations (East)



Figure 7: Conceptual Elevations (West)



Figure 8: Conceptual Elevations (North)



Figure 9: Conceptual Rendering

